



April 2021

Planning Justification Report

**Amendment to Zoning By-Law
No. 6593**

**Royal Living Development Group Inc.
705 & 713 Rymal Road East
City of Hamilton**



Contents

1.0 Introduction	4
2.0 Subject Lands	6
3.0 Proposed Development	9
3.1 Pre-Consultation	9
4.0 Planning Framework	12
4.1 Provincial Policy Statement, 2020	12
4.1.1 Policies	13
4.1.2 Conclusion	18
4.2 Provincial Growth Plan 2020	19
4.2.1 Policies	20
4.2.2 Conclusion	24
4.3 Urban Hamilton Official Plan 2013	24
4.3.1 Policies	25
4.3.2 Conclusion	39
4.4 Eleanor Neighbourhood Plan (Map 7505)	39
4.4.1 Policy	40
4.4.2 Conclusion	40
4.5 City of Hamilton Zoning Bylaw, No. 6593	40
4.5.1 Policy	40
4.5.2 Conclusion	42
5.0 Draft Zoning By-Law No. 6593, 1950 Amendment	44
6.0 Technical Reports	46
6.1 Urban Design Brief	46
6.2 Archaeological Assessment	46
6.3 Functional Servicing and Stormwater Management Report	47
6.4 Noise Impact Study	47
6.5 Transportation Impact Study	47
6.6 Transportation Demand Management Report	48
7.0 Planning Opinion and Conclusions	50

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text '1.0 Introduction' is centered in white.

1.0

Introduction

1.0 Introduction

Royal Living Development Group Inc. is the owner of lands legally described as Part of Lot 8 Concession 8 Geographic Township of Barton Historical County of Wentworth in the City of Hamilton, and located at 705 and 713 Rymal Road East, Hamilton.

Wellings Planning Consultants Inc. was retained by Royal Living Development Group (“the applicant”) to provide planning advice and to prepare and file an application to amend the City of Hamilton Zoning By-law No. 6593 (“Zoning By-law”).

The purpose of the application is to facilitate the development of the subject lands located on the north side of Rymal Rd. E., east of Upper Sherman Ave., and west of Miles St. The application proposes 41, two-storey street townhomes with surface parking.

Common element condominium and site plan applications will be filed at a later date to implement the proposed Zoning By-law Amendment.

This Planning Justification Report has been prepared in support of the application.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the image.

2.0

Subject Lands

2.0 Subject Lands

The subject lands are located in the Eleanor neighbourhood in the southwest area of Hamilton. The site is approximately 0.88 hectares (2.18 acres) in size, having approximately 61 metres of frontage on Rymal Rd. E., and a depth of approximately 147 metres. The location of the subject lands is shown on **Schedule 1** of this report.

The subject lands are currently composed of two individual, adjacent parcels, municipally known as 705 Rymal Rd. E. and 713 Rymal Rd. E. in the City of Hamilton. The subject lands are composed of two different zones;

- “AA” (Agricultural) on the North side of the property; and
- “C” (Urban Protected Residential, Etc.) on the South side of the property.

705 Rymal Rd. E. is a vacant parcel, containing moderate shrubs and greenery, formerly the location of a detached dwelling. There is a 2.5 metre noise wall to the west of this property installed as required for the development of the adjacent ‘No Frills’ commercial use.

713 Rymal Rd. E. contains a single-detached dwelling, set back from Rymal Rd. E., with a front and rear grassed area and minor shrubs and greenery throughout the site. There is significant tree and shrub coverage to the east of this property, providing a buffer between the adjacent residential land use.

Access to 705 Rymal Rd. E. and 713 Rymal Rd. E. is currently achieved by way of separate private driveways with full-turn access to Rymal Rd. E.

Surrounding uses of the subject lands include;

- **West:** ‘No Frills’ plaza, with additional restaurant and mixed-uses, zoned as G-1 (Designed Shopping Centre).
- **East:** Single-detached one and two-storey dwellings, zoned as C (Urban Protected Residential).
- **South:** Single-detached, two-storey and one-storey dwellings, as well as a place of worship (Eternal Spring United Church), zoned as C (Urban Protected Residential).
- **North:** Single-detached, two-storey residential uses, zoned as R-4/S-1658 (Small Lot Single Family Dwelling) and C (Urban Protected Residential).

Buildings along Rymal Rd. E. within eyesight of the subject lands do not exceed two-storeys, inclusive of both residential and commercial uses. To the south of Rymal Rd. E., is the Chappel East and Broughton West neighbourhoods, composed primarily of agricultural uses, with some residential and institutional uses.

The closest major intersection to the west of the subject lands is Upper Sherman Ave. and Rymal Rd. E., composed of the 'No Frills' plaza, townhouses, and an auto shop. The closest intersection to the east of the subject lands is Miles Rd/Eva St and Rymal Rd. E., composed of residential two-storey detached dwellings.

Rymal Rd. E. is classified as a Major Arterial on Schedule C: Functional Road Classification of the Urban Hamilton Official Plan ("UHOP"). It is also classified as a Secondary Corridor (Urban) on Schedule E: Urban Structure. The subject lands are situated along the potential future route of the Hamilton B.L.A.S.T. rapid transit line on Rymal Rd. E., as shown on Appendix B: Major Transportation Facilities and Routes of the UHOP.

The existing character of the neighbourhood may be described as a low-rise neighbourhood in transition, largely composed of single-detached homes with some recent townhouse developments and the recent addition of the 'No Frills' Commercial Plaza.

It is expected that many of the existing large-lot single-detached dwellings fronting Rymal Rd. E. will be redeveloped into higher density, more compact built-forms.



The subject lands are approximately 0.88 hectares (2.18 acres) and composed of two parcels, 705 and 713 Rymal Rd. E., Hamilton.

The subject lands are located adjacent to commercial and low-rise residential uses.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the image.

3.0

Proposed Development

3.0 Proposed Development

The applicant is proposing to construct six (6) blocks (buildings) of two-storey, street townhouses, providing a total of 41 residential units on the subject lands. The units will be distributed amongst distinct townhouse blocks (buildings) as follows;

- Building A: 6 units,
- Building B: 7 units,
- Building C: 6 units,
- Building D: 6 units,
- Building E: 8 units,
- Building F: 8 units.



A total of 93 parking spaces are proposed, 11 of which are dedicated visitor parking [one (1) visitor parking space is proposed as accessible]. This provides 2 parking spaces per townhouse unit, plus 0.25 visitor parking spaces per townhouse unit. A site plan package showing the proposed layout and configuration of the proposed condominium townhouse layout is included within **Schedule 2** of this report.

All of the 41-street townhouse units are proposed as three (3) bedroom units.

3.1 Pre-Consultation

The proposal has evolved since the original concept was first put forward for the lands at 705 Rymal Rd during the first pre-consultation meeting on May 30, 2018 (File: FC-18-057).

At that time, the applicant was proposing 35 townhouse units, between two (2) and three (3) storeys in height, with 79 parking spaces. Issues identified at this pre-consultation meeting included;

- *‘Alterations to the driveway access; and*
- *Development of a strong street presence along Rymal with enhanced elevations requested for townhomes visible from the street.’*

A second formal pre-consultation meeting was held with City staff on May 29, 2019 (File: FC-19-010) which included lands at 705 Rymal Rd. E. and 713 Rymal Rd. E.

At that time, the applicant was proposing 89 two (2) and three (3) storey townhouse units, with associated parking. Relevant issues identified at this pre-consultation meeting included;

- *Water servicing concerns;*
- *A road widening (allowance) of approximately 3.1 metres required; and*
- *Alternative design requested that reflects the intent of the Eleanor Neighbourhood Plan.'*

A final formal pre-consultation meeting was held with City of Hamilton staff on September 18, 2019 (File: FC-19-93).

The site plan of the subject property was generally consistent with the previously proposed development (modifications made as per the previous meeting comments). The development was proposed as a stacked townhouse product to enhance the density of development of the subject lands.

Most recently, the City issued an FC Waiver Agreement on October 16, 2020 waiving the requirement for another pre-consultation meeting on the basis that submission requirements were generally consistent with the previous Formal Pre-Consultation Documents. The FC Waiver Agreement containing the list of required documentation for the application is included with the submission.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the left side of the image.

4.0

Planning Framework

4.0 Planning Framework

There are a number of planning policy documents which apply to the subject lands. Namely, the Provincial Policy Statement 2020, the Growth Plan 2020, the Urban Hamilton Official Plan (“UHOP”), the Eleanor Neighbourhood Plan, and Zoning By-Law No. 6593 will be reviewed with respect to the proposed development.

4.1 Provincial Policy Statement, 2020



The Provincial Policy Statement, 2020 (PPS 2020) provides policy direction on matters of Provincial interest related to development and land use in Ontario. As a ‘policy-led’ planning system, the PPS 2020 establishes policies to support the enhancement of quality of life for all Ontarians through land-use planning and regulation.

The PPS 2020 is complemented by Provincial plans and locally-generated policies, recognizing that a ‘comprehensive, integrated, place-based and long-term planning system’ requires municipal official plans as a vehicle for the implementation of the PPS 2020 vision.

Policies of the PPS 2020 represent minimum standards, and the policies provide latitude for planning authorities to go beyond the minimum standards to address matters of importance to a specific community.

As required under the Section 3 of the Ontario *Planning Act*, all land use decisions must “be consistent with” the PPS.

Part IV (Vision) of the PPS sets out the Province’s vision for the wise management of land to meet the full range of current and future needs for the people of Ontario. Achieving efficient development patterns that make the best use of land, resources and public infrastructure is a foundational principle of the PPS 2020. Ensuring the health and safety of residents is another essential part of the Province’s vision. The PPS 2020 acknowledges the impacts of climate change, and the need for adaptation and response to these changes.

The vision of the PPS is to build communities as follows:

“Strong, liveable and healthy communities promote and enhance human health and social well being, are economically and environmentally sound and are resilient to climate change.”

To achieve strong, liveable and healthy communities, the PPS states as follows:

*“Efficient development patterns optimize the use of land, resources and public investment in infrastructure and public service facilities. These land use patterns **promote a mix of housing**, employment, recreation, parks and open spaces, and transportation choices that increase the use of active transportation and transit before other modes of travel. They support the financial well-being of the Province and municipalities over the long term, and minimize the undesirable effects of development, including impacts on air, water and other resources. They also permit better adaptation and response to the impacts of a changing climate, which will vary from region to region.”*

4.1.1 Policies

Managing and Directing Land Use

Policies contained in Section 1.1 of the PPS 2020 set out Provincial policies for managing and directing land use. With respect to this application, the following selected policies are of note;

1.1.1 *“Healthy, liveable and safe communities are sustained by:*

- a) *promoting **efficient development and land use patterns** which sustain the financial well-being of the Province and municipalities over the long term;*

The proposed development represents an efficient form of development by maximizing the use of existing municipal infrastructure (water and transportation) through the development of a compact built-form (i.e. street townhouses).

- b) *accommodating an appropriate affordable and **market-based range and mix of residential types** (including **single-detached**, additional residential units, **multi-unit housing**, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*

Of note, Policy 1.1.1 b) has specifically added single-detached and multi-unit housing to the range and mix of residential types which differs from the 2014 PPS. The proposed development

provides a market-based street townhouse product that contributes towards the diversification (mix) of housing in the neighbourhood.

- e) ***promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;***

Intensification is defined as;

“the development of a property, site or area at a higher density than currently exists through:

- a) redevelopment, including the reuse of brownfield sites;*
- b) the development of vacant and/or underutilized lots within previously developed areas;*
- c) infill development; and*
- d) the expansion or conversion of existing buildings.”*

Transit-support is defined as;

“in regard to land use patterns, means development that makes transit viable, optimizes investments in transit infrastructure, and improves the quality of the experience of using transit. It often refers to compact, mixed-use development that has a high level of employment and residential densities, including air rights development, in proximity to transit stations, corridors and associated elements within the transportation system. Approaches may be recommended in guidelines developed by the Province or based on municipal approaches that achieve the same objectives.”

The proposed development represents intensification at a higher density than current exists. 705 Rymal Rd. E is currently a vacant lot, while 713 Rymal Rd. E. accommodates one (1) single-detached dwelling. The proposed development is also a form of infill development.

Additionally, the proposed development is transit supportive as the development will be a compact built-form, with a higher-level of residential density than currently exists. The current transit level of service provided by HSR includes;

- Weekday headways between 15 and 30 minutes, and
- Weekend headways of 20-60 minutes.

As per the Transit Supportive Guidelines published by the Ministry of Transportation, the minimum suggested residential density for this level of service is 22 units (gross) per hectare (Figure 1). The residential density will be approximately 46.57 units per gross hectare (compared to the existing residential density of approximately 1.12 units per hectare), exceeding the suggested minimum density from the transit supportive guidelines, and providing

a density that is supportive of ‘Very Frequent Bus Service’ (i.e., one bus every 5 minutes with potential for LRT or BRT).

Transit service type	Suggested minimum density
Basic Transit Service (One bus every 20-30 minutes)	22 units per ha / 50 residents & jobs combined
Frequent Transit Service (One Bus every 10-15 minutes)	37 units per ha / 80 residents & jobs combined
Very Frequent Bus Service (One bus every 5 minutes with potential for LRT or BRT)	45 units per ha / 100 residents & jobs combined
Dedicated Rapid Transit (LRT/BRT)	72 units per ha / 160 residents & jobs combined
Subway	90 units per ha / 200 residents & jobs combined

Figure 1. Transit Supportive Guidelines, Ministry of Transportation, Ontario, 2012

Settlement Areas

Section 1.1.3 of the PPS provides direction for Settlement Areas. Settlement Areas are considered “*built up areas where development is concentrated and which have a mix of land uses.*”

1.1.3.1 “**Settlement Areas shall be the focus of growth and development.**”

1.1.3.2 “*Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a) **efficiently use land** and resources;
- b) *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- e) *support active transportation;*
- f) **are transit-supportive**, where transit is planned, exists or may be developed...”

The proposed development is located within a Settlement Area and is proposed to provide a built-form that efficiently utilizes land through the achievement of a higher residential density than currently exists, while maintaining the character of the neighbourhood.

As an infill form of development, the proposed development will also efficiently use existing water, hydro, road, transit and public service facilities which are already available and/or planned for along Rymal Rd. E.

The proposed development supports active transportation by providing a residential use adjacent to the 'No Frills' Plaza, permitting residents to walk to purchase groceries and obtain other services within the commercial plaza.

The proposed development is also transit-supportive. This is an important measure due to the planned Hamilton B.L.A.S.T. rapid transit system along Rymal Rd. E. as identified within Appendix B: Major Transportation Facilities and Routes of the UHOP. Lands in close proximity to such transit development projects should be appropriately intensified to ensure maximum utilization and efficiency of public transit infrastructure.

Housing

Section 1.4 of the PPS 2020 consists of provisions for housing which includes providing an appropriate range and mix of housing/densities and maintaining the ability to accommodate residential growth for a minimum of 15 years.

1.4.1 *"To provide for an **appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area** planning authorities shall:*

- a) ***maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development...***

The proposed street townhouse development will provide the City with additional housing options that contribute to the range, mix and densities of housing currently available. This will assist the City in maintaining their ability to accommodate the residential growth required over the 15-year time horizon.

Section 1.4.3 provides additional direction with respect to housing;

1.4.3 ***"Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:***

b) permitting and facilitating:

2. **all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;**
- c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*
- e) **requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.”**

The proposed development provides a form of residential intensification in an area having appropriate levels of infrastructure. The proposed development will efficiency use the existing, available infrastructure.

Additionally, the proposed development will be transit-supportive given the compact built-form with a density that will support existing and future planned public transportation.

Transportation Systems

The PPS 2020 specifically identifies density and a mix of uses as key components for efficient transportation systems in Policy 1.6.7.4;

- 1.6.7.4 “**A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.**”

The proposed development is well served by retail and commercial uses, allowing residents to limit their personal vehicle trips by using active or public transportation. Additionally, the proposed development is supportive of current and future public transit. Further, while the subject lands may not be located within a highly walkable neighbourhood at present (having a walk score of ‘40 – Car Dependent’), it is recognized that the neighbourhood is in a state of transition. As redevelopment and intensification occurs within the area, the proposed development, having a more compact built-form, will contribute to a more walkable community.

Transportation and Infrastructure Corridors

Section 1.6.8.3 provides that new development adjacent to existing and/or planned corridors and transportation facilities should be compatible and supportive of the purposes of the corridor;

1.6.8.3 *“Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified.*

New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.”

The proposed townhouse development will supply additional potential transit users to maximize the usage of current and planned public transportation along Rymal Rd. E. Additionally, the Transportation Impact Study identified that no improvements to the transportation network are triggered as a result of the proposed development, ensuring that the Major Arterial which supports freight and people movement is not negatively impacted by the proposed development.

The proposed development is therefore supportive of the long-term purposes of the Secondary Corridor.

Long -Term Economic Prosperity

Section 1.7.1(a) of the PPS 2020 recognizes the connection between long-term economic prosperity and residential uses.

1.7.1 *“Long-term economic prosperity should be supported by:*

- a) ***encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce”***

The proposed redevelopment will contribute to the long-term economic prosperity of the community. By providing more residential uses, especially in a compact built-form, the City is able to provide housing supply for the workforce. Providing an appropriate supply of housing within the urban boundary, which impacts the cost and convenience of living, is recognized by the Invest in Hamilton Economic Development website (City of Hamilton, 2021) as a contributing factor to economic development.

4.1.2 Conclusion

Having reviewed the PPS 2020 in its entirety, I am satisfied that the proposal is consistent with the PPS 2020.

The development provides an efficient use of the subject lands at an appropriate density, and the development contributes to the diversity of available housing stock necessary to support the municipality's investment in localized infrastructure, inclusive of transit. The moderate form of intensification will help to sustain a healthy, livable and safe community, contributing towards economic development and long-term prosperity within the City.

4.2 Provincial Growth Plan 2020

The proposal is subject to the provisions of the Provincial Growth Plan, 'A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020' ("Growth Plan").

The recently updated Growth Plan was approved under the authority of Section 7 of the Places to Grow Act, 2005, and came into effect on May 16, 2019, with most recent amendments coming into effect on August 28, 2020.

The Growth Plan provides policies for the management of population and employment growth and guides municipal decisions on a multitude of issues including, but not limited to, transportation, infrastructure planning, land-use planning, urban form, housing, natural heritage and resource protection.

This updated document acknowledges that since the Growth Plan was first introduced, development has shifted to a more compact development pattern, with a greater variety of housing options, while acknowledging that "there is still more work to do".

The Growth Plan manages development growth and environmental protection in the Greater Golden Horseshoe ("GGH"), requiring that **all planning decisions by respective authorities within the GGH "conform to" the Growth Plan.**



The Growth Plan builds upon the foundation set by the PPS by providing more specific land use policies that take precedence over the policies of the PPS, to the extent of any conflict.

Policies of the Growth Plan represent minimum standards, and the plan encourages decision-makers to go beyond minimum standards where appropriate.

The Growth Plan vision statement refers to having sufficient housing supply, reflective of the market demand within the GGH. It also makes a commitment to protect the Greenbelt while undertaking efforts to achieve *‘complete communities with access to transit, employment and housing.’*

The Growth Plan envisions a future of choices for travel with fast, convenient and affordable public transit, as well as *‘vibrant urban centres characterized by more compact development patterns to support climate change mitigation and adaptation, and provide diverse live, work and culture opportunities.’*

The Growth Plan provides guiding principles which form the foundation of the plan, and provide a greater understanding of the ‘why’ of the policies. Intensification, flexibility, and recognition of diversity are some of the principles that support complete communities with a range and mix of housing, mobility, and economic opportunities. Guiding principles of note include;

- ***“Support the achievement of complete communities that are designed to support healthy and active living and meet people’s needs for daily living throughout an entire lifetime.***
- ***Support a range and mix of housing options, including additional residential units and affordable housing, to serve all sizes, incomes, and ages of households.***
- ***Improve the integration of land use planning with planning and investment in infrastructure and public service facilities, including integrated service delivery through community hubs, by all levels of government.”***

4.2.1 Policies

Where and How to Grow

The Growth Plan includes policies that encourage development within settlement areas, the establishment of complete communities, and making more efficient use of vacant, underutilized lands and available infrastructure. The plan provides minimum intensification and density targets for individual communities of the GGH. As the Growth Plan states; *“building more compact greenfield communities reduces the rate at which land is consumed.”* With the intent to optimize existing urban land supply, the plan explicitly directs for an *‘intensification first’* approach to development, with less dependence on continuously expanding the urban area.

Section 2.2.1 speaks to managing the growth of municipalities within the GGH;

2.2.1. 2. *“Forecasted growth to the horizon of this Plan will be allocated based on the following:*

- a) ***the base majority of growth will be directed to settlement areas that:***
 - i. *have a delineated built boundary;*
 - ii) *have existing or planned municipal water and wastewater systems; and*

- iii) *can support the achievement of complete communities;*
- c) ***within settlement areas, growth will be focused in:***
 - i) *delineated built-up areas;*
 - ii) *strategic growth areas;*
 - iii) *locations with existing or planned transit, with a priority on higher order transit where it exists or is planned; and*
 - iv) *areas with existing or planned public service facilities;*
- d) *development will be directed to settlement areas, except where the policies of this Plan permit otherwise.”*

Further to the above, ‘Complete Communities’ are defined by the Growth Plan as;

“Places such as mixed-use neighbourhoods or other areas within cities, towns, and settlement areas that offer and support opportunities for people of all ages and abilities to conveniently access most of the necessities for daily living, including an appropriate mix of jobs, local stores, and services, a full range of housing, transportation options and public service facilities. Complete communities are age-friendly and may take different shapes and forms appropriate to their contexts.”

While the subject lands are not located within the built-boundary, the site is surrounded by the built-boundary delineated on Schedule G of the UHOP. Additionally, the ‘No Frills’ Plaza is an example of a recent development located outside of the built-boundary which was completed adjacent to the subject lands. The site also has existing water and wastewater services available along Rymal Rd. E. and the subject lands are already served by two (2) HSR bus routes. The proposed development resides along the future planned B.L.A.S.T. rapid transit corridor.

Additionally, the subject lands are designated as “Neighbourhoods” in the UHOP, where redevelopment and intensification is anticipated. The site is served by existing public service facilities, defined by the Growth Plan as including “social assistance, recreation, police and fire protection, health and educational programs, and cultural services”.

The proposed development supports the achievement of complete communities by contributing to the mix of uses in the neighbourhood and providing the new residents of the proposed development with convenient access to some daily necessities (i.e., groceries). The proposed development also contributes toward a full range of housing in the neighbourhood.

The Growth Plan requires local and regional municipalities to undertake integrated planning to manage forecasted growth that will:

- 2.2.1.3 c) ***“provide direction for an urban form that will optimize infrastructure, particularly along transit and transportation corridors, to support the achievement of complete communities through a more compact built form.”***

“Compact Built Form” communities are defined in the Growth Plan 2020 as:

“A land use pattern that encourages the efficient use of land, walkable neighbourhoods, mixed land uses (residential, retail, workplace, and institutional) all within one neighbourhood, proximity to transit and reduced need for infrastructure. Compact built form can include detached and semi-detached houses on small lots as well as townhouses and walk-up apartments, multi-storey commercial developments, and apartments or offices above retail.”

The proposed development makes efficient use of existing urban land and conforms to the ‘Compact Built Form’ definition provided in the Growth Plan by proposing a compact, appropriate residential use along an existing transit route and future rapid transit corridor.

The proposed development represents an efficient use of land that will utilize existing water, wastewater, transit and utilities along Rymal Rd. E. with an intensified residential use that is compatible with the surrounding neighbourhood.

The subject lands are also within walking distance of several convenient amenities such as the ‘No Frills’ Plaza, Eleanor Park and Billy Sherring Park. The definition specifically notes that ‘townhouses’ can also be a compact built-form.

Section 2.2.1.4 states that applying the policies of the Growth Plan will support the achievement of complete communities that;

- a) ***“feature a diverse mix of land uses, including residential and employment uses, and convenient access to local stores, services, and public service facilities;***
- c) ***provide a diverse range and mix of housing options, including additional residential units and affordable housing, to accommodate people at all stages of life, and to accommodate the needs of all household sizes and incomes;***
- d) ***expand convenient access to:***
 - i. ***a range of transportation options, including options for the safe, comfortable and convenient use of active transportation;***
- e) ***provide for a more compact built form and a vibrant public realm, including public open spaces.”***

The proposed development will contribute to the diverse mix of land uses in the area and provides a compact residential development adjacent to the ‘No Frills’ plaza.

Understanding that the existing residential character of the neighbourhood is predominantly single-detached residential, the proposed development contributes to the diversity of residential uses. The proposed development accomplishes this by providing street townhouses that are more affordable in comparison to the surrounding single-detached housing.

Residents of the proposed development will have convenient access to HSR bus routes, as well as to the future planned B.L.A.S.T. network along Rymal Rd. E.

The development will also meet the definition of 'Compact Built Form' as previously discussed, by providing street townhouses.

Housing

Section 2.2.6 of the Growth Plan provides direction on housing, as follows;

1. *“Upper and single-tier municipalities, in consultation with lower-tier municipalities, the Province, and other appropriate stakeholders, will:*
 - a) **Support housing choice through the achievement of the minimum intensification and density targets in this Plan, as well as the other policies of this Plan by:**
 - i. **Identifying a diverse range and mix of housing options and densities, including second units and affordable housing to meet projected needs of current and future residents;**
 - b) **Identify mechanisms, including the use of land use planning and financial tools, to support the implementation of policy 2.2.6.1 a);**
 - d) **Implement policy 2.2.6.1 a), b) and c) through official plan policies and designations and zoning by-laws.**
2. **Notwithstanding policy 1.4.1 of the PPS in implementing policy 2.2.6.1, municipalities will support the achievement of complete communities by:**
 - a) **Planning to accommodate forecasted growth to the horizon of this Plan;**
 - b) **Planning to achieve the minimum intensification and density targets in this Plan;**
 - c) **Considering the range and mix of housing options and densities of the existing housing stock; and**
 - d) **Planning to diversify their overall housing stock across the municipality.”**

Within the City of Hamilton context, along with many other municipalities in the GGH, the Greenbelt Plan, the Niagara Escarpment Plan, and the Growth Plan limit expansion beyond the existing urban boundary. This means that new residential development is unlikely to occur beyond the urban boundary in the near future. As a result, a large portion of new residential development will need to occur in the form of intensification within Settlement Areas.

The proposed townhouse development contributes to achieving the diversity and density targets set out in the Growth Plan. The proposed development additionally contributes to the mix and variety of housing options in the Eleanor neighbourhood, and provides a more-compact

built-form than the predominant (single-detached) housing types. This promotes ‘housing choice’ for the neighbourhood.

4.2.2 Conclusion

Having read the Growth Plan in its entirety, it is my opinion that the proposed development conforms to the Growth Plan 2020.

The proposal will facilitate an intensified development along a major transportation corridor, contribute to a complete community, provide compact development, and add to the mix of housing. The development results in the optimization of land by ensuring efficient utilization of existing infrastructure (transit, water, wastewater, hydro, gas, roads, etc.). The type, scale and transition of development is appropriate in the context of the Rymal Rd. E and surrounding residential.

The density of the development is supportive of current and planned transit. The proposed development also takes into account the existing character of the neighbourhood, and is compatible with surrounding land uses.

4.3 Urban Hamilton Official Plan

The Urban Hamilton Official Plan (“UHOP”) is the City’s land use planning document that guides development and balances the diverse interests of the community and serving as a significant document to implement the directives of Provincial policy.

General principles of the UHOP that are applicable to the proposed development include;

- *“compact and healthy urban communities that provide opportunities to live, work, play, and learn;*
- *balanced transportation networks that offer choice so people can walk, cycle, take transit, or drive, and recognize the importance of goods movement to our local economy;*
- *strategic and wise use of infrastructure services and existing built environment.”*

The City has also established Nine (9) Directions to Guide Development as part of the Growth-Related Integrated Development Strategy (GRIDS) program. The relevant directions to this application include;

“Direction #1

Encourage a compatible mix of uses in neighbourhoods that provide opportunities to live, work, and play.

Direction #2

Concentrate new development within existing built-up areas and within a firm urban boundary.

Direction #7

Maximize the use of existing buildings, infrastructure and vacant or abandoned land.”

The proposed development will provide compatible street townhouses to increase the mix and range of residential uses in the neighbourhood, as well as maintain new development within the existing urban boundary, as per ‘*Directions #1 and #2*’.

The proposed development will also maximize use of vacant, or otherwise underutilized land, within the urban boundary, as per ‘*Direction #7.*’

4.3.1 Policies

Schedules

The UHOP identifies the subject lands in various land use schedules as follows;

- Schedule C: Functional Road Classification – **Major Arterial**
- Schedule E: Urban Structure – **Secondary Corridor**
- Schedule E-1: Urban Land Use Designations - **Neighbourhoods**
- Schedule G: Local Housing Market Zones - **Hamilton Mountain**
- Appendix B: Major Transportation Facilities and Routes – **Along Potential Rapid Transit Line (B.L.A.S.T.)**
- Appendix F-4: Archaeological Potential – **Archaeological Potential**
- Appendix G: Boundaries Map (Built Boundary and Central Area Boundary) – **Within Urban Boundary, Outside of Built-Boundary**

Chapter B: Communities

Section B.2.4 of the UHOP provides direction regarding residential intensification, recognizing such as an important part of Hamilton’s growth strategy, and allowing the municipality to meet its respective growth and employment targets. The section describes intensification as follows;

“Intensification contributes to creating and maintaining vibrant neighbourhoods, nodes and corridors and can provide a wider range of housing types to meet the housing needs of Hamilton’s current and future population. This Plan supports the intensification of the existing built-up area of the City, with a focus on intensification of planned Urban Nodes and Urban Corridors described in Chapter E – Urban Systems and Designations.”

Chapter G provides a definition for intensification as follows:

“means the development of a property, site or area at a higher density than currently exists through:

- a) redevelopment, including the reuse of brownfield sites;*
- b) the development of vacant and/or underutilized lots within previously developed areas;*
- c) infill development; and*
- d) the expansion or conversion of existing buildings. (PPS, 2005)”*

The proposed development is a form of intensification, taking place on vacant and/or underutilized land, and infill development, thereby contributing towards vibrant neighbourhoods and a range of housing types as described within Section B.2.4 of the UHOP.

Section B.2.4.1 provides general support for residential intensification throughout Urban Nodes and Urban Corridors, as well as within Neighbourhoods;

2.4.1.2 “The City’s primary intensification areas shall be the Urban Nodes and Urban Corridors as illustrated on Schedule E – Urban Structure and as further defined in secondary plans and corridor studies for these areas, included in Volume 2.”

2.4.1.3 “The residential intensification target specified in Policy A.2.3.3.4 shall generally be distributed through the built-up area as follows:

- a) The Downtown Urban Growth Centre shall be planned to accommodate approximately 20% of the intensification target.**
- b) The Urban Nodes and Urban Corridors identified in Section E.2.0 - Urban Structure, excluding the Downtown Urban Growth Centre, shall be planned to accommodate approximately 40% of the residential intensification target.**
- c) 40% of the residential intensification target is anticipated to occur within the Neighbourhoods as illustrated on Schedule E – Urban Structure.”**

The subject lands are located within an Urban Corridor (Secondary Corridor) as per Schedule E of the UHOP, which is identified as a primary location for intensification.

Further, 40% of the City’s intensification is to be accommodated within Urban Nodes and Urban Corridors, as well as another 40% to be accommodated within Neighbourhoods. The subject lands are designated as both an Urban Corridor and Neighbourhoods, demonstrating clear alignment with the UHOP for residential intensification.

2.4.1.4 “Residential intensification developments shall be evaluated based on the following criteria:

- a) **a balanced evaluation of the criteria in b) through g), as follows;**
- b) **the relationship of the proposal to existing neighbourhood character so that it maintains, and where possible, enhances and builds upon desirable established patterns and built form;**

The proposed development will complement the existing neighbourhood. There is a mix of townhouses and detached housing units within close proximity to the subject lands, varying between one (1) and two (2) storeys in height. The proposed development will build upon the established residential area and built-form, with the height not exceeding two (2) storeys, and being similar in character to other townhouses along Rymal Rd E. and Upper Sherman Ave.

The proposed development is in keeping with the desired pattern of residential development, with design and transition elements which reduce impact on the neighbourhood to the North and to the East.

- c) **the development’s contribution to maintaining and achieving a range of dwelling types and tenures;**

The development will contribute to a range of dwelling types within the City. The proposed development will provide for condominium ownership for the 3-bedroom street townhouse units.

- d) **the compatible integration of the development with the surrounding area in terms of use, scale, form and character. In this regard, the City encourages the use of innovative and creative urban design techniques;**

Chapter G of the UHOP defines compatibility/compatible as;

“land uses and building forms that are mutually tolerant and capable of existing together in harmony within an area. Compatibility or compatible should not be narrowly interpreted to mean “the same as” or even as “being similar to”.”

The proposed development is compatible with surrounding uses for the following reasons;

- Providing an appropriate built-form to the surrounding residential land uses;
- Proposing a maximum height of two (2) storeys; and,
- Maintaining the ground-oriented character of the existing neighbourhood.

- e) *the development's contribution to achieving the planned urban structure as described in Section E.2.0 – Urban Structure;*

The subject lands are within a Secondary Corridor.

Section E.2.4 provides direction for Urban Corridors, stating the intent to;

“maintain and enhance the mixed use nature of the City’s corridors while recognizing that segments of individual corridors will differ in character and function and will evolve over time. The City’s corridors provide a significant opportunity for creating vibrant pedestrian and transit oriented places through investment in infrastructure, residential intensification, infill and redevelopment, and careful attention to urban design.”

The proposed development is a form of residential intensification which contributes towards the evolution of the Secondary Corridor as a future planned B.L.A.S.T. rapid transit line.

Further, Sub-section E.2.4.10 provides direction with respect to scale;

“The built form along the Urban Corridors shall generally consist of low to mid rise forms, but will vary along the length of the corridors with some areas permitted to accommodate high density and high rise built form... the Secondary Corridors shall generally accommodate retail and mixed use forms in small clusters along the corridors with medium density housing located between the clusters.”

The proposed development is considered low-density/low rise residential in the Neighbourhoods designation. This built-form conforms to the policies provided for Secondary Corridors.

Sub-section E.2.4.14 provides the following direction with respect to urban design;

“Urban Corridors shall provide a comfortable and attractive pedestrian experience.”

The proposed development is an appropriate scale (i.e. 2 storeys), and provides a modern, high-quality urban design along Rymal Rd. E. that will create a comfortable and attractive experience for pedestrians. The townhouse built-form will provide a human-scale pedestrian experience.

- f) *infrastructure and transportation capacity; and,*

A Functional Servicing Report (FSR) and Stormwater Management Report was prepared by ACII Group Inc., dated March 29, 2021. The report ‘demonstrates that the proposed development meets the required design criteria and is serviceable, in compliance with the City Standards.’

Regarding transportation capacity, Paradigm Transportation Solutions Inc. prepared a Traffic Impact Study (TIS) and a Transportation Demand Management (TDM) Report dated April 2021 for the proposed development. The TIS concluded that “no improvements are triggered by the proposed development.” The TDM Report also identified that some TDM measures are already included (i.e. internal site sidewalks linking to municipal sidewalks and minimal personal vehicle trip generation), and that additional TDM measures may be reviewed with respect to the landscaping plan and other features during the Site Plan Approval process.

g) the ability of the development to comply with all applicable policies.

All applicable policies have been identified within this report to ensure compliance of the proposed development within the UHOP. Additional applicable policies, including intensification and Neighbourhood policies, are reviewed below.

B.2.4.2 of the UHOP provides residential intensification policies specific to the Neighbourhoods designation;

2.4.2.1 “Residential intensification within lands designated Neighbourhoods identified on Schedule E-1 - Urban Land Use Designations shall comply with Section E.3.0 – Neighbourhoods Designation.

2.4.2.2 When considering an application for a residential intensification development within the Neighbourhoods designation, the following matters shall be evaluated:

a) the matters listed in Policy B.2.4.1.4;

These matters have been evaluated above.

b) compatibility with adjacent land uses including matters such as shadowing, overlook, noise, lighting, traffic, and other nuisance effects;

As per the definition of compatibility, and the justification previously provided in evaluating policy B.2.4.1.4 d), the proposed development is, in my opinion, compatible with adjacent uses.

Noise mitigation measures have been identified within the Environmental Noise Impact Study prepared by dBA Acoustical Consultants Inc., dated April 2021. The measures include provisions for A/C units, window, door and wall construction specifications, and warning clauses registered on title.

Additionally, “no [traffic/transportation] improvements are triggered by the proposed development” as per the TIS.

Any required lighting will be reviewed at Site Plan Approval to reduce/mitigate any lighting impacts.

There are no concerns of overlook given a maximum proposed height of two (2) storeys.

c) the relationship of the proposed building(s) with the height, massing, and scale of nearby residential buildings;

The proposed built-form is ground related with a height, scale and massing that is compatible with nearby single-detached residential and the existing adjacent commercial.

The townhouse development at 1619 Upper Sherman Ave. (the corner of Upper Sherman Ave. and Rymal Rd. E.) is a similar built form (two storey street townhouses). Additionally, to the east of the site, within the Eleanor neighbourhood, there is a three (3) storey street townhouse development (829 Rymal Rd. E.).

The proposed development provides a height, scale and massing that is appropriate in consideration of the surrounding context and location of the subject lands along a Major Arterial and Secondary (Urban) Corridor.

d) the consideration of transitions in height and density to adjacent residential buildings;

The proposed height and density of the street townhouses is consistent with the low-density category. The height of the proposed development is equal to the height of residential uses to the North, and one (1) storey higher than the height of the residential land use to the East.

It is my opinion that the transition in height and density with the adjacent residential and commercial is appropriate.

- e) *the relationship of the proposed lot(s) with the lot pattern and configuration within the neighbourhood;*

The lot pattern of the proposed street townhouses will provide, in most cases, abutting backyards to existing development. The proposed street townhouses will have a similar lot pattern to 1619 Upper Sherman Ave. and 829 Rymal Rd. E. projects noted above.

- f) *the provision of amenity space and the relationship to existing patterns of private and public amenity space;*

Each townhouse unit will have its own private rear yard amenity area. As noted above, the rear yards of the townhouses adjacent to residential will, for the most part, abut existing rear yards.

- g) *the ability to respect and maintain or enhance the streetscape patterns including block lengths, setbacks and building separations;*

Given the orientation of the development with townhouse blocks/units flanking Rymal Road E., there will be minimal impact on the public realm and the existing streetscape patterns. Townhouse blocks will range between six (6) and eight (8) units per block, with setbacks and building separations that are similar to the aforementioned townhouse developments.

The minimum distance between buildings will be 2.4 metres. The townhouses adjacent to Rymal Rd. E. will consist of upgraded facades.

- h) *the ability to complement the existing functions of the neighbourhood;*

The proposed development will complement the existing functions of the neighbourhood by providing a quality redevelopment with a new residential use that is compatible with surrounding residential uses. The development will complement the neighbourhood and support existing commercial and transit.

- i) *the conservation of cultural heritage resources; and,*

There are no cultural heritage resources that would be affected by the proposed development.

j) *infrastructure and transportation capacity and impacts.*”

There were no adverse impacts to the infrastructure and transportation systems identified within the FSR and Stormwater Management, TIS or TDM reports.

The analysis above demonstrates that intensification in the form of the proposed development is appropriate and in conformity with the policies of the UHOP.

3.2 of the UHOP speaks to the importance of housing within the City of Hamilton;

“Housing is fundamental to the economic, social and physical well-being of Hamilton’s residents and communities. Housing is a basic human need and is the central place from which people build their lives, nurture their families and themselves, and engage in their communities. Housing needs to change and evolve as social, demographic, and economic conditions change. The long term sustainability of communities is based on building a diverse, flexible housing stock today to meet changing needs at both household and community levels. To ensure that housing is available for all residents with a wide variety of needs, there must be a sufficient supply of housing with a range of housing types, forms, tenures, densities, affordability levels, and housing with support services.”

Section 3.2.1 provides relevant urban housing goals as the following;

3.2.1.1 “Provide for a range of housing types, forms, and densities to meet the social, health and well-being requirements of all current and future residents.

3.2.1.6 Increase the mix and range of housing types, forms, tenures, densities, affordability levels, and housing with supports throughout the urban area of the City.”

The proposed development will contribute toward the range of housing types in the neighbourhood by providing a built-form and density that is less predominant within the Eleanor Neighbourhood. The proposed development will therefore increase the mix of housing, providing more options for residents.

Chapter C: City Wide Systems and Designations

The proposed development will contain individual housing units with a common element condominium road running through the centre of the property. Chapter C, Section 4.5.5, is therefore a relevant policy with respect to condominium roads;

“A common element condominium road shall be considered as a public road for the purposes of the Zoning By-law.”

The above policy clarifies the intent to treat condominium roads established through a common element plan of condominium similar to public right of ways.

As the subject lands are located on a Secondary Corridor (Rymal Rd. E), Section 4.4.8 of the UHOP applies;

“The City shall evaluate the potential to establish rapid transit within the Primary and Secondary Corridors identified on Schedule E – Urban Structure, and the proposed corridors identified as Potential Rapid Transit Lines on Appendix B – Major Transportation Facilities and Routes.”

The City has identified a potential future Rapid Transit Line (B.L.A.S.T.) along Rymal Rd E. (the ‘S’ line). The S line would run along the frontage of the proposed development.

Chapter E: Urban Systems and Designations

The goals of the urban systems and land use designations include:

- c) *“**Develop compact, mixed use urban environments** that support transit and active transportation.*
- d) *Develop complete communities where people can live, work, learn, and play.*
- e) *Plan and designate lands for a range of housing types and densities, taking into account affordable housing needs.*
- f) *Promote and support appropriate residential intensification throughout the urban area and focused in Urban Nodes and Urban Corridors.*
- g) *Recognize that Hamilton’s neighbourhoods are **stable, not static.**”*

The proposed development is a compact built-form that is transit-supportive. The proposed development also supports active transportation by being located adjacent to the ‘No Frills’ Plaza, permitting residents to walk to obtain groceries, as well as other convenience amenities.

The proposed development further contributes toward complete communities and a range of housing types. The proposed development is also an appropriate form of residential intensification and supportive of Urban Corridors.

Additionally, the UHOP specifically acknowledges that while Neighbourhoods are stable, they are not static. This recognizes that redevelopment forms part of the natural evolution of

Neighbourhoods. Intensification, particularly along a designated Urban Corridor with a future planned rapid transit line, should be expected in neighbourhood evolution.

The subject lands have been identified within Schedule E as a Secondary Urban Corridor. As such, the following policies are relevant to the proposed development.

2.1 *“Hamilton’s urban structure shall be a node and corridor structure guided by the following general principles:*

- a) **Nodes and corridors are the focus of reurbanization activities** (i.e. population growth, private and public redevelopment, and infrastructure investment).
- d) *Nodes and corridors provide a vibrant pedestrian environment and facilitate active transportation through careful attention to urban design.*
- e) **Nodes and corridors evolve with higher residential densities and mixed use developments to achieve their planned functions and support transit.”**

The nodes and corridors are noted as areas with a reurbanization focus inclusive of population growth and development. The proposed development represents reurbanization. The proposed development will also contribute towards a vibrant pedestrian environment by incorporating high-quality facades close to the frontage of the property. Further, the subject lands are located adjacent to the ‘No Frills’ Plaza which will facilitate active transportation for some daily needs (i.e., groceries).

Urban Corridors, identified within Schedule E of the UHOP, are described as;

2.4 *“Urban Corridors follow several major streets throughout the City and will continue to link our communities together as they have in the past. Corridors have traditionally **included a mix of uses** providing locations for businesses and housing as well as physical and social focal points for the adjacent neighbourhoods. The intent of this Plan is to **maintain and enhance the mixed use nature of the City’s corridors** while recognizing that segments of individual corridors will differ in character and function and will evolve over time. The City’s corridors provide a significant opportunity for creating vibrant pedestrian **and transit oriented places through investment in infrastructure, residential intensification, infill and redevelopment, and careful attention to urban design.”***

The proposed development will provide housing that will enhance the mixed-use nature of the corridor by providing an intensified residential use adjacent to a commercial plaza.

The proposed development will also promote transit-orientated development in support of current and planned transportation along Rymal Rd. E. and in close proximity to the subject lands.

The proposed development will provide an appropriate form of residential intensification on an infill lot.

The function of the Urban Corridors includes the following;

- ***‘Be the location for a range of higher density land uses along the corridor, including mixed uses where feasible’***

The subject lands are within an Urban Corridor and will serve the function by promoting a higher density land use than is currently existing on the subject lands.

The scale of the Urban Corridors includes the following;

- ***‘The built form consisting of low to mid rise forms... with Secondary Corridors generally accommodating retail and mixed use - with medium density housing located between the clusters;***
- ***Be a focus for intensification; and***
- *Specific to Secondary, evolve to an increasing proportion of multiple storey, mixed use clusters.’*

The proposed development represents an intensified low-rise built-form.

The design of the Urban Corridors includes the following;

- *‘Provide a comfortable and attractive pedestrian experience;*
- *New development shall **respect the existing built form** of adjacent neighbourhoods - providing a **gradation in building height; and***
- *New development shall locate and be designed to minimize the effects of shadowing and overview on properties adjacent’*

The low density scale of development and two (2) storey height provides a design that respects the existing built-form of adjacent land uses.

Section 2.6 of Chapter E provides general direction with respect to the ‘Neighbourhoods’ designation. The subject property is designated as Neighbourhoods as per Schedule E-1 of the UHOP. The applicable direction policies are paraphrased as follows;

- *‘An all-encompassing element representing the concept of complete communities - containing a mix of low, medium, and high-rise residential areas;*
- ***Stable not static*** – *physical change and evolution through renovations, infill development, and commercial invigoration are to be expected at a range of scales and densities provided the **intensification is compatible with and respects the built form and character** of the surrounding neighbourhood;*
- ***Primarily consist of residential uses*** and *complementary facilities and services intended to serve the residents – including retail and restaurants;*
- *Provide opportunity for a **full range of housing forms, types and tenure; and***
- *Applications for development and residential intensification shall be reviewed in **consideration of the local context** and shall be permitted in accordance with Sections B.2.4 – Residential Intensification.’*

The proposed development will contribute towards a mix of housing available within the Eleanor Neighbourhood.

The residential intensification policies under Section B.2.4 were previously addressed.

Section 3.1 of Chapter E provides the following policy goals for neighbourhoods;

- 3.1.1 ***“Develop compact, mixed use, transit-supportive, and active transportation friendly neighbourhoods.***
- 3.1.2 *Develop neighbourhoods as part of a **complete community**, where people can live, work, shop, learn, and play.*
- 3.1.3 *Plan and designate lands for a **range of housing types and densities**, taking into account affordable housing needs.*
- 3.1.4 *Promote and support design which enhances and respects the character of existing neighbourhoods while at the same time allowing their ongoing evolution.*
- 3.1.5 ***Promote and support residential intensification of appropriate scale and in appropriate locations throughout the neighbourhoods.”***

The proposed development will support complete communities and will accommodate appropriate residential intensification within the local context. The proposed development is also public and active-transit supportive.

Section 3.2 describes the intended function, scale, and design of neighbourhoods, paraphrased as follows;

- *‘Function as complete communities, including the full range of residential dwelling types and densities;*
- ***Uses permitted*** on lands designated Neighbourhood include;

- *residential dwellings, including second dwelling units and housing with supports...;*
- *Existing character of established Neighbourhoods maintained;*
- **Residential intensification shall enhance and be compatible with the scale and character of the existing residential, designed to be safe, efficient, pedestrian oriented, and attractive, and shall comply with the following criteria:**
 - *Garages, parking areas, and driveways along the public street shall not be dominant. Surface parking between a building and a public street (excluding a public alley) shall be minimized; and*
 - *Adequate and direct pedestrian access and linkages to community facilities/services and local commercial uses shall be provided;*
 - **Development shall improve existing landscape features and overall landscape character of the surrounding area; and**
 - **Development shall comply with Section B.3.3 – Urban Design Policies and all other applicable policies; and**
- **Rezoning applications for new uses shall be evaluated to ensure compatibility with surrounding land uses.'**

The proposed street townhouses are a permitted use within the Neighbourhoods designation, and the level of intensification contemplated is compatible with the scale and character of existing residential uses.

The proposed development will also comply with the applicable criteria provided in Section E.3.2.7 above, improving the existing landscape, reducing the dominance of driveways along the arterial, and providing pedestrian linkages into the development. The proposed development will also comply with Section B.3.3 urban design policies.

Section 3.3 provides the following general direction for residential uses;

- 3.3.1 *“Lower density residential uses and building forms shall generally be located in the interiors of neighbourhood areas with higher density dwelling forms and supporting uses located on the periphery of neighbourhoods on or in close proximity to major or minor arterial roads.”*
- 3.3.2 **“Development or redevelopment adjacent to areas of lower density shall ensure the height, massing, and arrangement of buildings and structures are compatible with existing and future uses in the surrounding area.”**

The Neighbourhoods designation provides for low, medium and high density residential uses. Low density residential uses shall generally be located in the interior of Neighbourhoods with higher density built-forms on the periphery of Neighbourhoods, or in close proximity to major or minor arterials.

In this case, the subject lands are situated at the periphery of the Neighbourhood. The proposed development is considered low density residential given the proposed use (i.e. street townhouses) and density of development less than 60 units per net residential hectare. The proposed development will have a density of 52.9 units per net residential hectare.

With surrounding residential uses being one (1) to two (2) storeys, the proposed development provides for modest intensification on the periphery of the Neighbourhood.

Section 3.4 provides the function, scale and design of Low Density Residential uses;

Function

- 3.4.1 *“The preferred location for low density residential uses is within the interior of neighbourhoods.*
- 3.4.2 *Low density residential areas are characterized by lower profile, **grade-oriented built forms** that generally have direct access to each unit at grade.*
- 3.4.3 **Uses permitted** in low density residential areas include single-detached, semi-detached, duplex, triplex, and **street townhouse dwellings.**”

Scale

- 3.4.4 *“For low density residential areas the **maximum net residential density shall be 60 units per hectare.***
- 3.4.5 *For low density residential areas, **the maximum height shall be three storeys.**”*

Design

- 3.4.6 *“Development in areas dominated by low density residential uses shall be designed in accordance with the following criteria:*
- a) **Direct access from lots to adjacent to major or minor arterial roads shall be discouraged.**
 - b) **Backlotting along public streets and in front of parks shall be discouraged...**
 - c) **A mix of lot widths and sizes compatible with streetscape character; and a mix of dwelling unit types and sizes compatible in exterior design, including character, scale, appearance and design features; shall be encouraged.**”

As noted above, the proposal is located on the periphery of the Neighbourhood. The proposed development will provide a grade-oriented built-form, with direct street access via the condominium road to each unit at grade. Street townhouses is a permitted use under the UHOP Neighbourhoods designation and considered a low density use.

Regarding the scale of the development, the net residential density of the site is approximately 52.9 units per net hectare, and the maximum height of the proposed development is 2 storeys, both of which are below the maximums prescribed for low density residential within the UHOP. The proposed development is designed to achieve a density and height less than the maximum permitted within the UHOP to address the character of the Neighbourhood and ensure compatibility with adjacent land uses.

The proposed development also meets the design requirements of the low density residential designation by prohibiting direct access from individual lots to Rymal Rd. E. (Major Arterial, Secondary Corridor). Additionally, there will be no “backlotting” along public streets as referenced by the UHOP.

4.3.2 Conclusion

Having read the UHOP and the applicable policies for the subject lands, **it is my opinion that the proposed development conforms to the UHOP.**

The proposed redevelopment conforms to the goals, objectives and policy directions of the UHOP in terms of intensification, housing options and residential permissions, built form, and compatibility of development with surrounding land uses.

The proposed development is located within the Neighbourhoods designation, and conforms to the function, scale, and design policies of the Low-Density Residential use. The proposed development is also located along a Secondary Corridor where intensification of residential uses is encouraged to support the function of Urban Corridors.

The proposed development meets the general function, design, and scale intent of the Secondary Corridor, while having appropriate regard for the existing character and built-form of adjacent uses and the surrounding neighbourhood. The proposed development also satisfied the policies of the UHOP with respect to residential intensification.

4.4 Eleanor Neighbourhood Plan

The Eleanor Neighbourhood Plan was approved by Council November 27 of 1973. The Neighbourhood Plan encompasses lands within Stone Church Rd E., Upper Gage Ave., Rymal Rd E. and Upper Sherman Ave.

Policy F. 1.2.7 of the Urban Hamilton Official Plan states;

"Neighbourhood plans are policies adopted by council resolution and do not form part of the Official Plan. Any proposal for development or redevelopment must conform to the designations, and policies in the Neighbourhood Plan."

4.4.1 Policy

The subject lands are designated as *'Single and Double,'* which does not permit townhouse development. The neighbourhood plan also shows a proposed road connection from Eleanor Ave. to Halo St. through the subject lands.

The proposed development will require a re-designation of the subject lands to *'Attached Housing.'* The proposed development will also require that the proposed road through the site be removed. The proposed road on the Neighbourhood Plan is not required as the recently completed housing development to the North of the subject lands already has two access points via Brenda St. and Dulgaren St.

It is my planning opinion that the proposed road connection shown in the Neighbourhood Plan provides no value from a planning perspective for existing residents or the proposed development.

4.4.2 Conclusion

Having reviewed the Eleanor Neighbourhood Plan, it is my opinion that the proposed modification to the Eleanor Neighbourhood Plan to re-designate the subject lands to *'Attached Housing'* represents good planning and is compatible with surrounding land-uses.

4.5 Hamilton Zoning Bylaw No. 6593

The City of Hamilton approved the Zoning By-Law No. 6593 ("ZBL") on July 25 of 1950. While most properties in the former municipalities within institutional, industrial, parks and open space uses have transitioned to Hamilton's new Zoning By-Law No. 05-200, residential uses for the subject lands remain regulated by the ZBL.

4.5.1 Policy

The proposed development (street townhouses) is a residential use, as defined by the ZBL, below;

""Residential" shall mean designed, adapted or used as a home or residence of a family or families, and shall include an apartment hotel, dwelling, multiple dwelling, maisonette dwelling, townhouse dwelling, street townhouse dwelling, hostel, hotel,

lodging house, tourist home, tourist camp, trailer camp and summer camp, foster home, residential care facility, short term care facility, a home for elderly persons, but shall not include any institutional use; and "Residential District" means and includes any "A", "AA"..."

Section 2.2.A vii d) defines a "Dwelling, Street Townhouse" as;

"A townhouse dwelling,

- a) in which all single family dwelling units front on a public street, and*
- b) which do not share any yards, driveways, common open space and parking and manoeuvring space."*

Both properties are currently split-zoned, being partially zoned "C" (Urban Protected Residential) and partially "AA" (Agricultural).

Street Townhouse Dwellings are not a permitted use in the "AA" and "C" Zones. Therefore, an amendment to the ZBL to an RT-30-XX Zone is required. The performance standards for the RT-30 Zone are provided within Section 10F of the Zoning By-Law. Relevant provisions are provided in the table below.

Section #	Regulations	Required	Provided
10F.(2)(a)	Permitted Use	A Street Townhouse Dwelling or Street Townhouse Dwellings	Street Townhouse Dwelling
10F.(3)	Building Height	3 storeys/11 metres	2 storey/9.6 m
10F.(4)(a)	Front Yard	A front yard of a depth of not less than 6.0 metres (19.69 feet)	3.2 m
10F.(4)(b)	Rear Yard	A rear yard of a depth not less than 7.5 metres (24.61 feet)	6.0 m
10F.(4)(c)(ii)	Side Yard	2.0 metres (6.56 feet) for a Street Townhouse Dwelling, not exceeding two storeys in height	3.0 m
10F.(5)(b)	Distance Between Buildings	not exceeding two storeys in height, of not less than 3.5 metres (11.48 feet)	Minimum 2.4 m
10F.(6)(i)	Lot Area	A lot area not less than 180.0 square metres (1,937.56 square feet)	150 m ²

10F.(6)(ii)	Lot Width	A width of not less than 6.0 metres (19.69 feet)	6.0 m
10F.(9)	Dwelling Unit Placement	Not more than eight single family dwelling units shall be attached in a continuous row	Maximum 8 dwelling units in a row
Table 1-1(e)	Parking	1 space per dwelling unit	2 per dwelling
Table 2-5	Visitor Parking	0.30 of a space per dwelling unit	0.25 space per dwelling unit (0.25 x 41 = 11 spaces)
18A.(7)	Parking Stall Size	2.7 metres wide and 6.0 metres long	2.8 m wide x 5.8 m long (aligns with ZBL. 05-200)

4.5.2 Conclusion

The “C” and “AA” Zones do not permit street townhouse dwellings, therefore a re-zoning to a RT-30 Zone with special provisions is required to permit the proposed development. The special provisions required are minor in nature and for the most part maintain the general intent of the “RT-30” Zone.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the left side of the image.

5.0

**Draft Zoning
By-Law**

5.0 Draft Zoning By-Law

A Draft Zoning By-Law Amendment (ZBLA) has been provided within **Schedule 3** of the report in support of the proposed development application. The ZBLA, consistent with the UHOP, establishes the condominium road as being the same as a public road. The ZBLA proposes site specific amendments to the RT-30 Zone to permit a modest decrease in the front yard and rear yard depths, minimum distance between buildings, minimum lot area size per unit, minimum visitor parking, and parking stall size.

More specifically, the ZBLA proposes a 3.2 m minimum front yard setback, a 6.0 m minimum rear yard setback, a 2.4 m minimum building separation distance, a 150 m² minimum lot area per unit, a 0.25 visitor parking rate, and an adjustment to parking stall sizes to 2.8 m by 5.8 m.

The ZBLA also proposes to increase the residential parking requirement from one (1) space to two (2) spaces per dwelling unit, and provide a modest reduction in building height (i.e. 2 storeys).

It is anticipated that further discussions with City Planning and Zoning staff will be required on the ZBLA prior to finalization to ensure all aspects of the parent ZBL are met.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the image.

6.0

Technical Studies

6.0 Technical Reports

In support of the application, several technical studies and reports have been completed. These studies and reports form part of, and are integral to, my planning opinion on the application. A summary of the technical reports is as follows;

6.1 Urban Design Brief

Adesso Design was retained to complete the Urban Design Brief (UDB) for the proposed development.

The conclusions of Adesso Design's UDB dated April 2021 can be summarized as follows. The proposed development will;

- *“Provide quality housing within Hamilton’s urban boundary that contributes to a Complete Community;*
- *Have building masses and designs with high quality materials that enhance and complement the existing neighbourhoods;*
- *Be compatible with planned future development in the surrounding area;*
- *Create comfortable and safe pedestrian circulation.”*

Adesso Design addressed the requirements of the City’s Urban Design Guidelines and the UHOP.

6.2 Archaeological Assessment

Detritus Consulting Ltd. (‘Detritus’) conducted a Stage 1-2 Archaeological Assessment (within two respective reports for each property). These reports are dated November 2018.

The Stage 1 assessment found that some of the properties had a moderate to high potential for the identification and recovery of archaeological resources, resulting in the recommendation to proceed with a Stage 2 assessment.

The subsequent Stage 2 assessment resulted in the identification of no archaeological findings. The reports concluded that *“no further archaeological assessment of the Study Area is recommended.”*

6.3 Functional Servicing and Stormwater Management Report

ACIII Group Inc. prepared an FSR and Storm Water Management Report dated March 29, 2021. The Report examined the serviceability of the proposed development with respect to wastewater, water, site grading and stormwater management.

The Report concluded that the proposed townhouse development *“meets the required design criteria and is serviceable for sanitary and storm sewers, stormwater management, water distribution and site grading, in compliance with the City of Hamilton Standards.”*

6.4 Noise Impact Study

dBa Acoustical Consulting Inc. prepared a Noise Impact Study dated April 2021 to determine the noise impact from Rymal Rd. E and Upper Sherman Ave. vehicular traffic. Rail and Aircraft were not identified as a noise concern.

The results of the study determined that some noise impact is anticipated from Rymal Rd. E. and Upper Sherman Ave. vehicular traffic. The report provided six (6) recommendations to mitigate the impacts of noise, including;

- Provisions for A/C Units;
- Window, Door, and Wall construction recommendations;
- Types “B” & “C” Warning Clause for specific residential Units are required and Registered on Title (R1-R4 Residential Units, inclusive);
- A letter from the window company be issued to confirm STC values for all proposed windows to be installed and an Acoustical Certificate confirming STC Values achieved;
- Certification from a qualified acoustical consultant that the required noise control measures have been incorporated into the builder’s plans prior to issuance of a building permit; and
- Certification from a qualified acoustical consultant that the required control measures have been properly installed prior to an occupancy permit.

6.5 Transportation Impact Study

Paradigm Transportation Solutions Limited (Paradigm) conducted a TIS dated April 2021.

While the TIS noted some existing localized congestion occurring at Rymal Rd E and Eva St./Miles Rd. and Rymal Rd. E. and Upper Sherman Ave., the TIS concluded that *“no improvements are triggered by the proposed development.”* The proposed development is expected to generate 20 AM peak hour trips and 27 PM peak hour trips.

The TIS noted that localized congestion is already occurring at Rymal Rd. E and Eva St./Miles Rd. for the Northbound approach during weekday peak hours. The intersection of Rymal Rd. E and Upper Sherman Ave. is projected to operate at LOS E for the Southbound through movement and the Eastbound left turn movement. Additionally, the intersection of Rymal Rd. E and Eva St./Miles Rd. is forecast to operate at LOS F for the Northbound approach and LOS E for the Westbound left turn movement.

While not triggered by the proposed development, the TIS recommended that the City may wish to consider the following remedial measures;

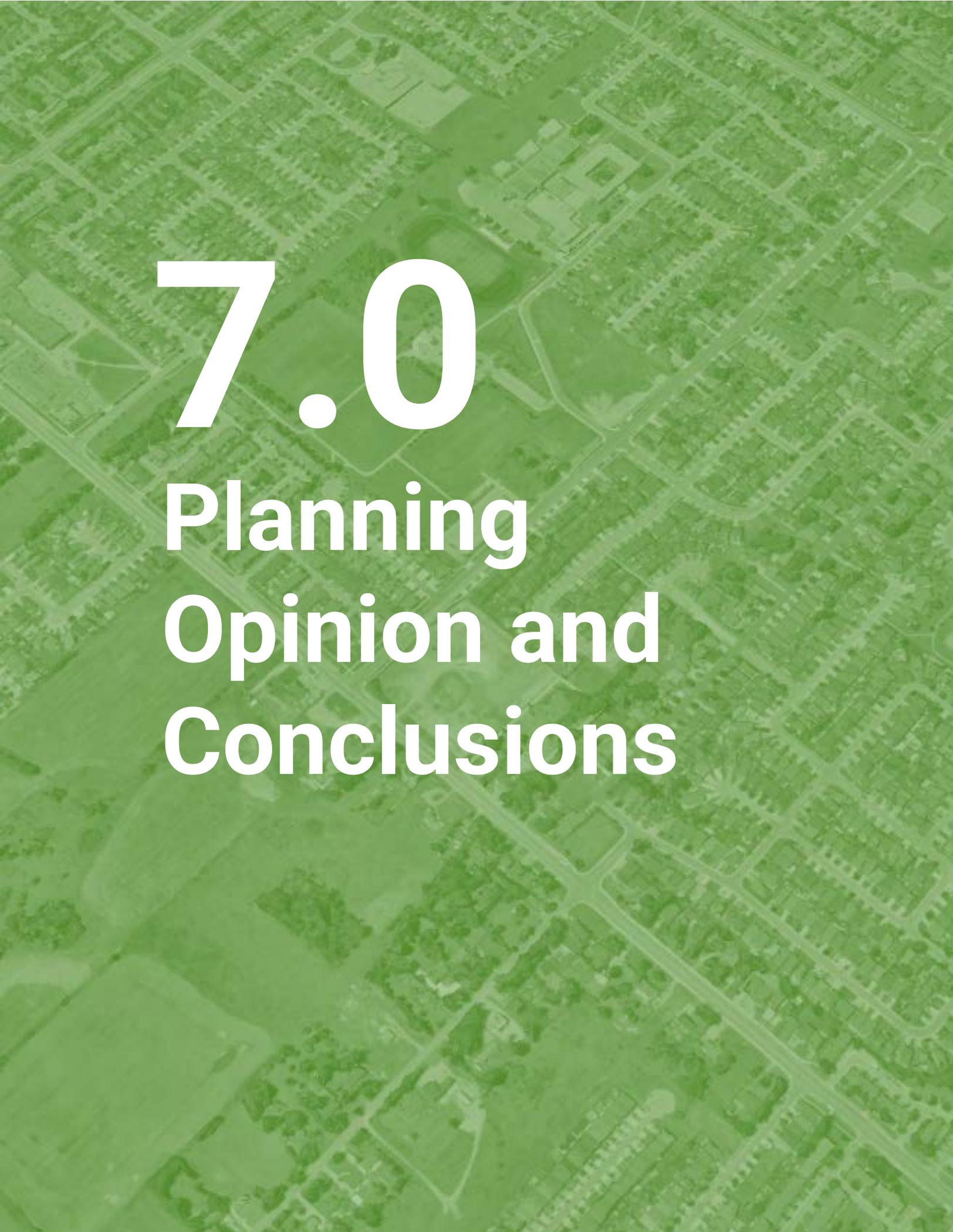
- An actuated coordinated timing plan and optimized signal timings for the Rymal Rd. E and Upper Sherman Ave. intersection; and
- Eastbound right turn lane (instead of shared), the addition of a permitted/protective phasing for Northbound approach, and optimized signal timings for the Rymal Rd E and Eva St/Miles Rd intersection.

6.6 Transportation Demand Management Report

Paradigm conducted a TDM Report dated April 2021, included as a section within the TIS.

The Report proposes several TDM measures (sidewalks and minimal vehicle trips) identified by the City of Hamilton's TDM Guide for Development. The report also included additional recommendations that the applicant will consider during Site Plan Approval;

- Additional amenities on the landscaping plan to enhance the pedestrian realm and to prioritize pedestrians;
- All on-site sidewalks should be well-lit and should conform to the City of Hamilton's design standards and Ontario AODA standards; and
- Encourage residents to utilize sustainable transportation options for travel to/from the development (transit/cycling/walking) - inclusive of a welcome package that outlines the available transit routes and active transportation options for the area.

An aerial photograph of a city grid, showing streets, buildings, and green spaces, overlaid with a semi-transparent green filter. The text is centered on the left side of the image.

7.0

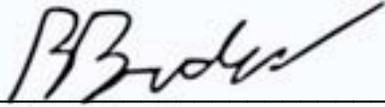
Planning Opinion and Conclusions

7.0 Planning Opinion and Conclusions

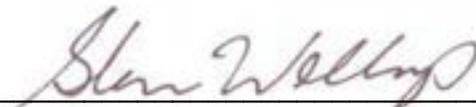
The application to amend the ZBL and modify the Eleanor Neighbourhood Plan to facilitate the development and residential intensification of the subject lands represents good planning and should be approved for the following reasons:

- 1) The proposed redevelopment is consistent with the PPS by providing an efficient use of the subject lands at an appropriate density and contributes to the diversity of available housing stock necessary to support localized infrastructure and transit. The modest form of intensification proposed will help to sustain a healthy, livable, and safe community, contributing towards economic development and long-term prosperity within the City of Hamilton.
- 2) The proposed development conforms to the Provincial Growth Plan by making more efficient use of vacant, underutilized lands through a compact built form that is supportive of complete communities and public transit. The proposal appropriately balances the prioritization of intensification and the character of the neighbourhood to provide an optimal and compatible development.
- 3) The proposed redevelopment conforms to the goals, objectives, and policy directions of the UHOP in terms of intensification, housing options, built form, and compatibility of development with surrounding land uses. The proposed development is located within the Neighbourhoods designation, and conforms to the function, scale, and design policies for Low-Density Residential. The proposed development is also located along a Secondary Corridor where intensification and residential uses are encouraged to support the function of Urban Corridors. The proposed development satisfies the general function, design, and scale intent of the Secondary Corridor, while having appropriate regard for the existing character and built-form of adjacent uses and the surrounding neighbourhood. The proposed development also satisfies the tests provided within Sections B.2.4.1.4 and B.2.4.2.2 of the UHOP to support residential intensification.
- 4) The modification of the subject lands from 'Single and Double' to 'Attached Housing' within the Eleanor Neighbourhood Plan is appropriate to facilitate the proposed development.
- 5) The ZBLA to rezone the subject lands to a new RT-30 with special provisions will bring the ZBL into conformity with the UHOP by providing a form of residential intensification that contributes towards complete communities and supports the intent of the Neighbourhoods and Secondary Corridor designations.
- 6) The supporting technical reports and studies undertaken conclude that the proposed development is will not result in any technical issues and/or adverse impacts.

Yours truly,
WELLINGS PLANNING CONSULTANTS INC.



Brad Bruder, BES



Glenn J. Wellings, MCIP, RPP





PROJECT:
Rymal Townhomes
 Hamilton, Ontario

705 and 713 Rymal Road East

PROJECT No:
 --

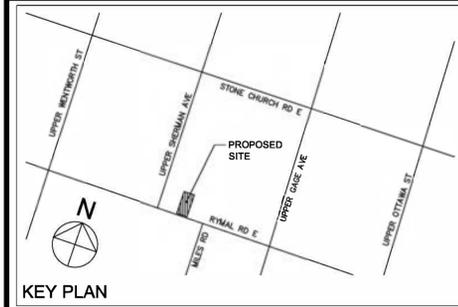
DATE:
April 2021

SCALE:
1:3800

Wellings Planning Consultants Inc.

513 Locust Street, Unit B - Burlington, Ontario
 t - 905-681-1769 e - Glenn@WellingsPlanning.ca

Schedule 1



Note: THIS SITE PLAN IS BASED ON AND MUST BE READ IN CONJUNCTION WITH TOPO PLAN PREPARED BY A.T. McLAREN LIMITED. KNYMH ACCEPTS NO RESPONSIBILITY FOR ACCURACY OR COMPLETENESS OF DATA SUPPLIED.

TOPOGRAPHIC SURVEY OF PART OF LOT 8 CONCESSION 8 GEORGIC TOWNSHIP OF BARTON IN THE CITY OF HAMILTON

GENERAL NOTES

- ALL WORK INVOLVED IN THE CONSTRUCTION, RELOCATION, REPAIR OF MUNICIPAL SERVICES FOR THE PROJECT SHALL BE TO THE SATISFACTION OF THE DIRECTOR OF PLANNING AND CHIEF PLANNER, PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT.
- FIRE ROUTE SIGNS AND 3-WAY FIRE HYDRANTS SHALL BE ESTABLISHED TO THE SATISFACTION OF THE CITY FIRE DEPARTMENT AND AT THE EXPENSE OF THE OWNER.
- MAIN DRIVEWAY DIMENSIONS AT THE PROPERTY LINE BOUNDARIES ARE PLUS OR MINUS 7.5 m UNLESS OTHERWISE STATED.
- ALL DRIVEWAYS FROM PROPERTY LINES FOR THE FIRST 7.5 M SHALL BE WITHIN 5% MAXIMUM GRADE, THEREAFTER, ALL DRIVEWAYS SHALL BE WITHIN 10% MAXIMUM GRADES.
- THE APPROVAL OF THIS PLAN DOES NOT EXEMPT THE OWNER'S BONDED CONTRACTOR FROM THE REQUIREMENTS TO OBTAIN THE VARIOUS PERMITS/APPROVALS NORMALLY REQUIRED TO COMPLETE A CONSTRUCTION PROJECT, SUCH AS, BUT NOT LIMITED TO THE FOLLOWING:
 - BUILDING PERMIT
 - SEWER AND WATER PERMITS
 - ROAD CUT PERMITS
 - RELOCATION OF SERVICES
 - APPROACH APPROVAL PERMITS
 - COMMITTEE OF ADJUSTMENT
 - ENCROACHMENT AGREEMENTS (IF REQUIRED)
- ABANDONED ACCESSES MUST BE REMOVED AND THE CURB AND BOULEVARD RESTORED WITH SOO AT THE OWNER'S EXPENSE TO THE SATISFACTION OF THE CORRIDOR MANAGEMENT SECTION, PUBLIC WORKS DEPARTMENT.
- FOR VISIBILITY TRIANGLES AT THE VEHICULAR ACCESS POINTS, THE FOLLOWING NOTE TO BE PROVIDED:

"5 METRE BY 5 METRE VISIBILITY TRIANGLES IN WHICH THE MAXIMUM HEIGHT OF ANY OBJECTS OR MATURE VEGETATION IS NOT TO EXCEED A HEIGHT OF 0.70 METRE ABOVE THE CORRESPONDING PERPENDICULAR CENTRELINE ELEVATION OF THE ADJACENT STREET."
- ALL SIGNS MUST COMPLY WITH SIGN BY-LAW No. 10-197

BUILDING BLOCK	AREA (m ²) GROUND FLOOR	NET RESIDENTIAL HECTARE = 8805m ² (lot area) + 1056m ² (street) = 7749m ² (0.775 ha)
A	527.1	NET RESIDENTIAL DENSITY = 41 units / 0.775 ha = 52.9 units per hectare
B	615.7	
C	527.3	
D	527.3	
E	703.4	
F	703.4	

UNIT TYPE	AREA (m ²) GROUND FLOOR	AREA (m ²) SECOND FLOOR	AREA (m ²) TOTAL
INTERIOR - TYPE 1	87.7	91.0	178.7
INTERIOR - TYPE 2	88.0	91.4	179.4
END - TYPE 3	87.9	91.0	178.9
END - TYPE 4	87.7	90.3	178.0

RT-30 Districts Zone

Section #	Regulations	Required	Provided
10F (2)(a)	Permitted Use	A Street Townhouse Dwelling or Street Townhouse Dwellings	Street Townhouse Dwellings
10F (3)	Building Height	3 storeys/11 metres	2 Storey / 9.6 m
10F (4)(a)	Front Yard	A front yard of a depth of not less than 6.0 metres (19.69 feet)	3.25 m
10F (4)(b)	Rear Yard	A rear yard of a depth not less than 7.5 metres (24.61 feet)	6.025 m
10F (4)(c)(i)	Side Yard	2.0 metres (6.56 feet) for a Street Townhouse Dwelling, not exceeding two storeys in height	3.0 m
10F (5)(b)	Distance Between Buildings	not exceeding two storeys in height, of not less than 3.5 metres (11.48 feet)	Minimum 2.41m
10F (6)(i)	Lot Area	A lot area not less than 180.0 square metres (1,937.56 square feet)	155 m ²
10F (6)(ii)	Lot Width	A width of not less than 5.0 metres (19.69 feet)	6.007 m
10F (9)	Dwelling Unit Placement	Not more than eight single family dwelling units shall be attached in a continuous row	Maximum 8 dwelling units in a row
Table 1-1(e)	Parking	1 space per dwelling unit	2 per dwelling
Table 2-5	Visitor Parking	0.25 space per dwelling unit (0.25 x 41 = 11 spaces)	0.25 space per dwelling unit
18A (7)	Parking Stall Size	2.7 metres wide and 6.0 metres long	2.8 m wide x 5.8 m long

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK

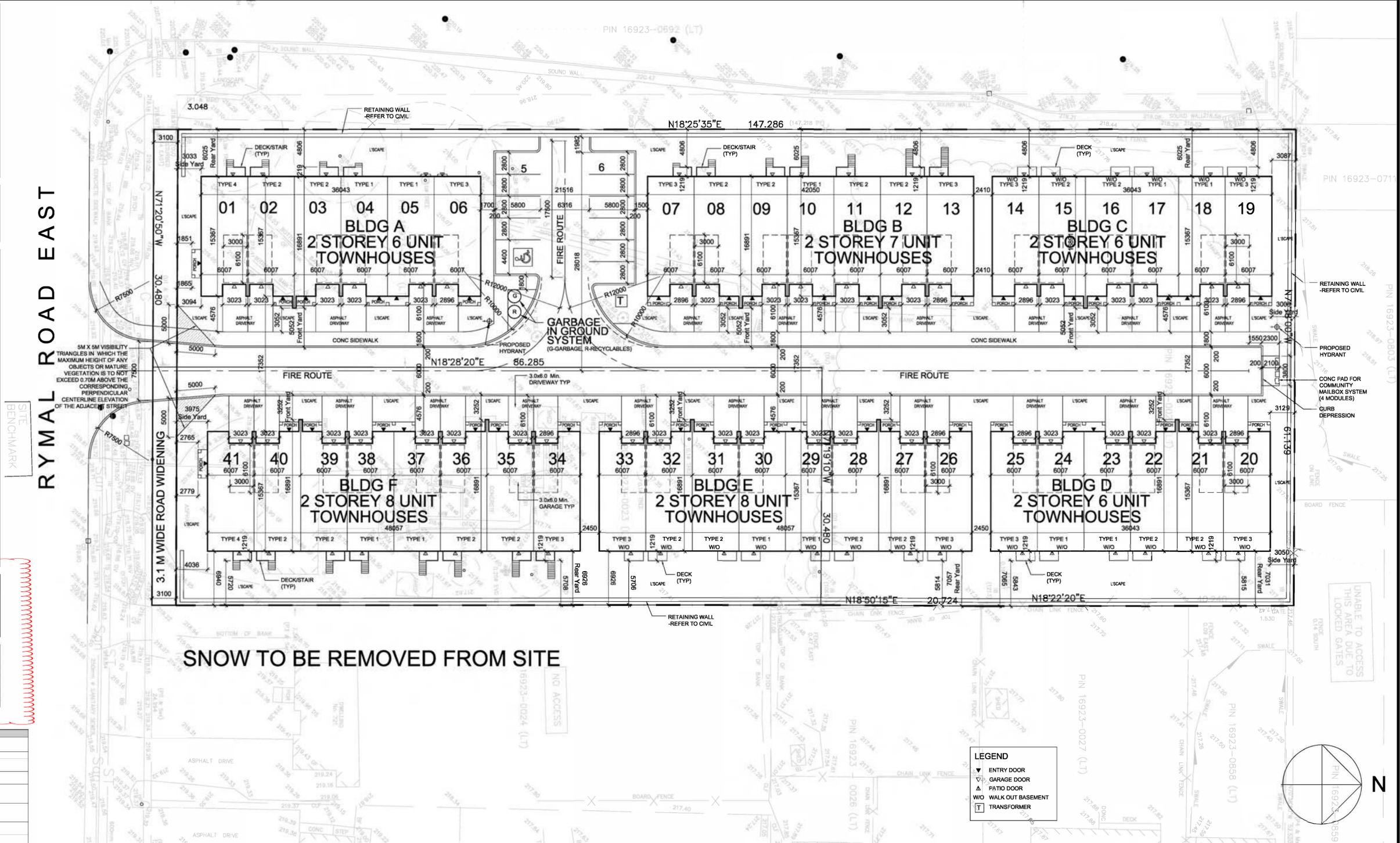
THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK

KEY TO DETAIL LOCATION

NO. DETAIL NUMBER
NO. DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:

NOT FOR CONSTRUCTION WITHOUT PERMIT



RYMAL ROAD EAST
SITE BENCHMARK

SNOW TO BE REMOVED FROM SITE

LEGEND

- ▼ ENTRY DOOR
- ▽ GARAGE DOOR
- △ PATIO DOOR
- W/O WALK OUT BASEMENT
- T TRANSFORMER

1 SITE PLAN
SP1 1:250

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK

THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK

KEY TO DETAIL LOCATION

NO. DETAIL NUMBER
NO. DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:

NOT FOR CONSTRUCTION WITHOUT PERMIT

DRAWING SETS ISSUED	No.	DATE	BY
CLIENT REVIEW	3	27.09.2019	WH
CLIENT REVIEW	4	12.03.2020	WH
CLIENT REVIEW	5	08.09.2020	WH
SUBMISSION	6	10.09.2020	WH
OPA and Rezoning Applications	7	22.03.2021	

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY

PROFESSIONAL ASSOCIATION OF ARCHITECTS

PRZEMYSŁAW MYSZKOWSKI
LICENCE 7884

KNYMH
ARCHITECTURE • SOLUTIONS

KNYMH INC.
1006 SKYVIEW DRIVE • SUITE 101
BURLINGTON, ONTARIO • L7P 0V1
T 905.639.6595
F 905.639.0394

www.knymh.com info@knymh.com

RYMAL TOWNHOMES

705 and 713 RYMAL ROAD EAST
HAMILTON, ONTARIO

DRAWING SHEET TITLE:
SITE PLAN

DRAWING SCALE:
AS NOTED

PROJECT NUMBER:
19039

DRAWN BY: CHECKED BY:
DRAWING SHEET NUMBER:
SP1

DRAWING VERSION:
PLOT DATE:
April 14, 2021

DRAWING LEGEND

SD1	VINYL SIDING -TYPE 1
BR1	BRICK -TYPE 1
STN	STONE
EPS1	EPS -TYPE 1
EPS2	EPS -TYPE 2
AT	ALUMINUM TRIM
AG	ALUMINUM GUTTER
AS	ASPHALT SHINGLE
PS	PRECAST SILL
AF	ALUMINUM FASCIA
SC	SOLDIER COURSE



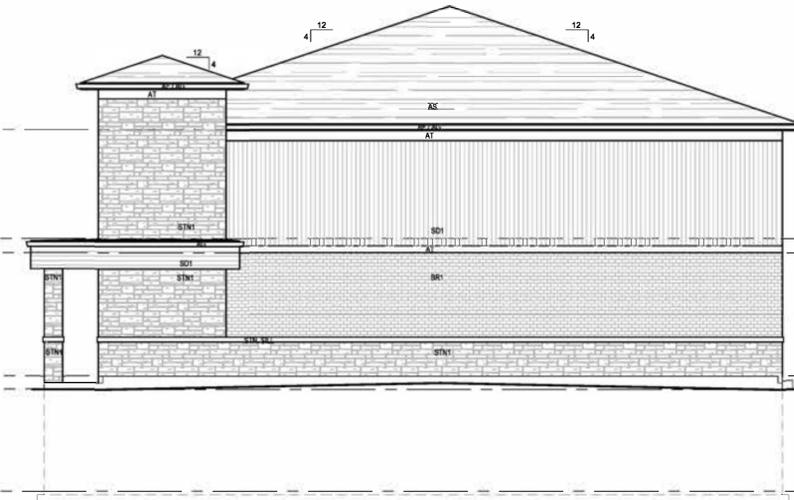
UNIT 01
TYPE 4



UNIT 01 TYPE 4 UNIT 02 TYPE 2 UNIT 03 TYPE 2 UNIT 04 TYPE 1 UNIT 05 TYPE 1 UNIT 06 TYPE 3

2 SOUTH ELEVATION
A300 1:75

1 EAST ELEVATION
A300 1:75



UNIT 06
TYPE 3



UNIT 06 TYPE 3 UNIT 05 TYPE 1 UNIT 04 TYPE 1 UNIT 03 TYPE 2 UNIT 02 TYPE 2 UNIT 01 TYPE 4

4 NORTH ELEVATION
A300 1:75

3 WEST ELEVATION
A300 1:75

THIS DRAWING IS THE PROPERTY OF KNYMH ARCHITECTS INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF KNYMH ARCHITECTS INC.

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK.

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY.

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK.

THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK.

KEY TO DETAIL LOCATION

No.	DETAIL NUMBER
No.	DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:
NOT FOR CONSTRUCTION WITHOUT PERMIT

DRAWING SETS ISSUED	No.	DATE	BY
CLIENT REVIEW	3	27.09.2019	WH
CLIENT REVIEW	4	12.03.2020	WH
CLIENT REVIEW	5	08.09.2020	WH
SUBMISSION	6	10.09.2020	WH
OPA and Rezoning Applications	7	22.03.2021	

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY



KNYMH
ARCHITECTURE • SOLUTIONS

KNYMH INC.
1006 SKYVIEW DRIVE • SUITE 101
BURLINGTON, ONTARIO • L7P 0V1
T 905.639.6595
F 905.639.0394
www.knymh.com info@knymh.com

RYMAL TOWNHOMES
705 and 713 RYMAL ROAD EAST
HAMILTON, ONTARIO

BUILDING - A

DRAWING SHEET TITLE: ELEVATIONS	
DRAWING SCALE: AS NOTED	PROJECT NUMBER: 19039
DRAWN BY: CHECKED BY: DRAWING VERSION:	DRAWING SHEET NUMBER: A300
PLOT DATE: March 22, 2021	

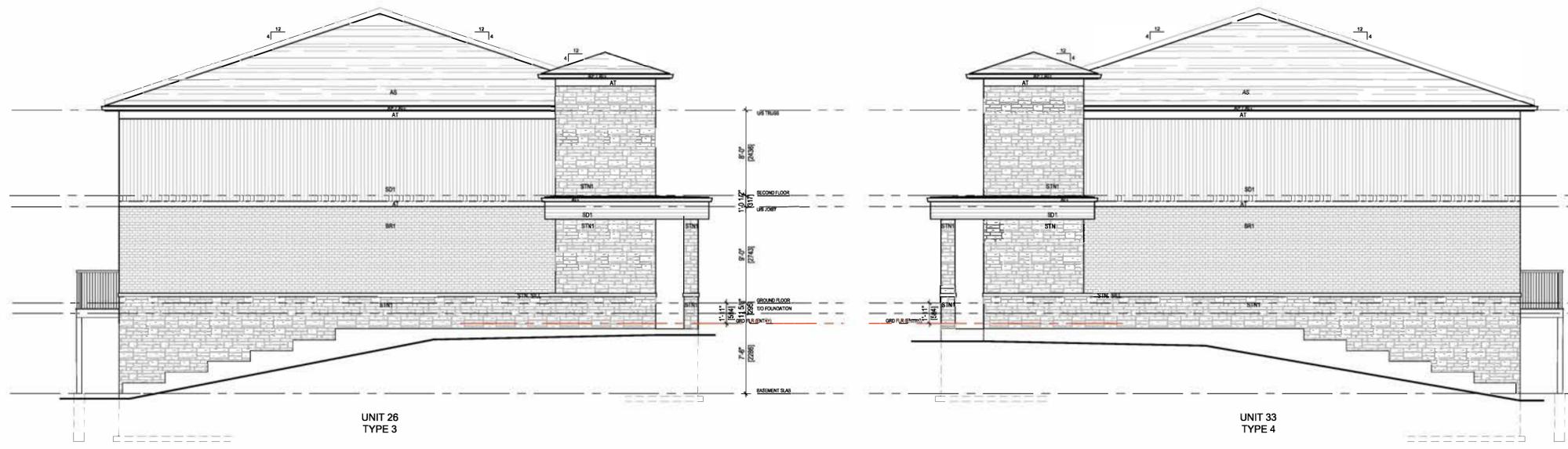
ALL PREVIOUS ISSUES OF THIS DRAWING ARE SUPERSEDED

DRAWING LEGEND

- SD1 VINYL SIDING -TYPE 1
- BR1 BRICK -TYPE 1
- STN STONE
- EPS1 EPS -TYPE 1
- EPS2 EPS -TYPE 2
- AT ALUMINUM TRIM
- AG ALUMINUM GUTTER
- AS ASPHALT SHINGLE
- PS PRECAST SILL
- AF ALUMINUM FASCIA
- SC SOLDIER COURSE



1 WEST ELEVATION
A300 1:75



2 NORTH ELEVATION
A300 1:75

3 SOUTH ELEVATION
A300 1:75

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK

THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK

KEY TO DETAIL LOCATION

No. DETAIL NUMBER
No. DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:
NOT FOR CONSTRUCTION WITHOUT PERMIT

DRAWING SETS ISSUED	No.	DATE	BY
CLIENT REVIEW	3	27.09.2019	WH
CLIENT REVIEW	4	12.03.2020	WH
CLIENT REVIEW	5	08.09.2020	WH
SUBMISSION	6	10.09.2020	WH
OPA and Rezoning Applications	7	22.03.2021	

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY



KNYMH
ARCHITECTURE • SOLUTIONS

KNYMH INC.
1006 SKYVIEW DRIVE • SUITE 101
BURLINGTON, ONTARIO • L7P 0V1
T 905.639.6595
F 905.639.0394
www.knymh.com info@knymh.com

RYMAL TOWNHOMES

705 and 713 RYMAL ROAD EAST
HAMILTON, ONTARIO

BUILDING - E

DRAWING SHEET TITLE: ELEVATIONS	
DRAWING SCALE: AS NOTED	PROJECT NUMBER: 19039
DRAWN BY: CHECKED BY:	DRAWING SHEET NUMBER: A300
DRAWING VERSION:	PLOT DATE: March 22, 2021

TRUSTAMP 2021 03 22 12:55:50 PM FILEPATH: C:\2019\19039 - Rymlal Living - Rymlal Standard Townshtk - Drawings\A300 - Schematics\Drawings\BLDG ELEVATIONS Elevation.dwg

DRAWING LEGEND

SD1	VINYL SIDING -TYPE 1
BR1	BRICK -TYPE 1
STN	STONE
EPS1	EPS -TYPE 1
EPS2	EPS -TYPE 2
EPS3	EPS -TYPE 2
AT	ALUMINUM TRIM
AG	ALUMINUM GUTTER
AS	ASPHALT SHINGLE
PS	PRECAST SILL
AF	ALUMINUM FASCIA
SC	SOLDIER COURSE



1 EAST ELEVATION
A301 1:75

T:\PROJECTS\2021-03-22-23\23-50-P4\FILEPATH\02\02018\19039 - RyMAL Living - RyMAL Standard Townsh. Drawing\04 - Schematic Design\FLOOR ELEVATIONS Elevation.dwg

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK.

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY.

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK.

THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK.

KEY TO DETAIL LOCATION

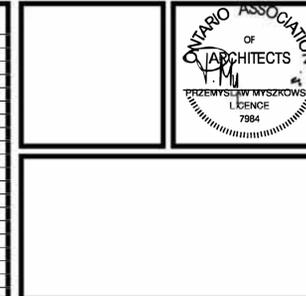
No.	DETAIL NUMBER
No.	DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:

NOT FOR CONSTRUCTION WITHOUT PERMIT

DRAWING SETS ISSUED	No.	DATE	BY
CLIENT REVIEW	3	27.09.2019	WH
CLIENT REVIEW	4	12.03.2020	WH
CLIENT REVIEW	5	08.09.2020	WH
SUBMISSION	6	10.09.2020	WH
OPA and Rezoning Applications	7	22.03.2021	

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY



KNYMH
ARCHITECTURE • SOLUTIONS

KNYMH INC.
1006 SKYVIEW DRIVE • SUITE 101
BURLINGTON, ONTARIO • L7P 0V1
T 905.639.6595
F 905.639.0394
www.knymh.com info@knymh.com

RYMAL TOWNHOMES

705 and 713 RYMAL ROAD EAST
HAMILTON, ONTARIO

BUILDING -E

DRAWING SHEET TITLE:	
ELEVATIONS	
DRAWING SCALE:	PROJECT NUMBER:
AS NOTED	19039
DRAWN BY: CHECKED BY:	DRAWING SHEET NUMBER:
	A301
DRAWING VERSION:	PLOT DATE:
	March 22, 2021

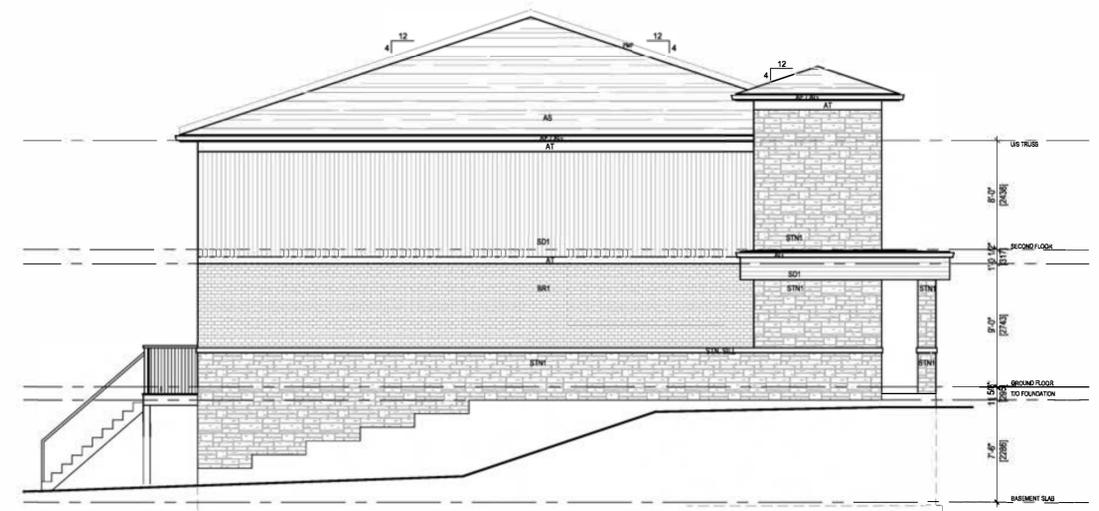
ALL PREVIOUS ISSUES OF THIS DRAWING ARE SUPERSEDED

DRAWING LEGEND

SD1	VINYL SIDING - TYPE 1
BR1	BRICK - TYPE 1
STN	STONE
EFS1	EFS - TYPE 1
EFS2	EFS - TYPE 2
AT	ALUMINUM TRIM
AG	ALUMINUM GUTTER
AS	ASPHALT SHINGLE
PS	PRECAST SILL
AF	ALUMINUM FASCIA
SC	SOLIDER COURSE



1 WEST ELEVATION
A300 1:75



2 NORTH ELEVATION
A300 1:75



3 SOUTH ELEVATION
A300 1:75

CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS AND JOB CONDITIONS BEFORE PROCEEDING WITH WORK.

ALL DRAWINGS MAY BE SUBJECT TO CHANGE DUE TO COMMENTS FROM MUNICIPAL DEPARTMENTS AND OTHER AGENCIES WITH AUTHORITY.

ALL DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF THE ARCHITECTS AND MUST BE RETURNED AT THE COMPLETION OF THE WORK.

THE CONTRACTOR WORKING FROM DRAWINGS NOT SPECIFICALLY MARKED FOR CONSTRUCTION MUST ASSUME FULL RESPONSIBILITY AND BEAR COSTS FOR ANY CORRECTIONS OR DAMAGES RESULTING FROM HIS OR HER WORK.

KEY TO DETAIL LOCATION

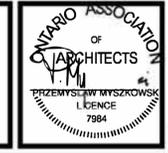
No.	DETAIL NUMBER
No.	DRAWING SHEET NUMBER

BUILDING PERMIT NUMBER:

NOT FOR CONSTRUCTION WITHOUT PERMIT

DRAWING SETS ISSUED	No.	DATE	BY
CLIENT REVIEW	3	27.09.2019	WH
CLIENT REVIEW	4	12.03.2020	WH
CLIENT REVIEW	5	08.09.2020	WH
SUBMISSION	6	10.09.2020	WH
OPA and Rezoning Applications	7	22.03.2021	

REVISIONS TO DRAWING	No.	DATE (DD.MM.YY)	BY



KNYMH
ARCHITECTURE • SOLUTIONS

KNYMH INC.
1006 SKYVIEW DRIVE • SUITE 101
BURLINGTON, ONTARIO • L7P 0V1
T 905.639.6595
F 905.639.0394
www.knymh.com info@knymh.com

RYMAL TOWNHOMES

705 and 713 RYMAL ROAD EAST
HAMILTON, ONTARIO

BUILDING - F

DRAWING SHEET TITLE: ELEVATIONS	
DRAWING SCALE: AS NOTED	PROJECT NUMBER: 19039
DRAWN BY: CHECKED BY:	DRAWING SHEET NUMBER: A300
DRAWING VERSION:	PLOT DATE: March 22, 2021

TIMESTAMP: 2021-03-22 12:56:26 PM FILEPATH: C:\2019\19039 - RyMAL Townhomes - RyMAL Logo - RyMAL Standard Template - Drawing\A300 Schematic Design\A300 ELEVATIONS F elev.dwg

Authority: Item ,
Report (PED19XXX)
CM:
Ward:

Bill No.

CITY OF HAMILTON

BY-LAW NO.

**To Amend Zoning By-law No. 6593
Respecting Lands Located at 705 and 713 Rymal Road East**

WHEREAS, the *City of Hamilton Act, 1999*, Statutes of Ontario, 1999 Chap.14, Schedule. C. did incorporate, as of January 1st, 2001, the municipality "City of Hamilton";

AND WHEREAS, the City of Hamilton is the successor to certain area municipalities, including the former area municipality known as "The Corporation of the City of Hamilton", and is the successor of the former regional municipality, namely, "The Regional Municipality of Hamilton-Wentworth";

AND WHEREAS, the *City of Hamilton Act, 1999* provides that the Zoning By-laws and Official Plans of the former area municipalities and the Official Plan of the former regional municipality continue in force in the City of Hamilton until subsequently amended or repealed by the Council of the City of Hamilton;

AND WHEREAS, the Council of the Corporation of the City of Hamilton passed Zoning By-law No. 6593 (Hamilton) on the 25th day of July 1950, which By-law was approved by the Ontario Municipal Board by Order, dated the 7th day of December 1951, (File No. P.F.C. 3821); and,

AND WHEREAS, this By-law is in conformity with the Urban Hamilton Official Plan.

NOW THEREFORE the Council of the City of Hamilton enacts as follows:

1. That Sheet No. E38d of the District Maps, appended to and forming part of Zoning By-law No. 6593 (Hamilton) is amended, as follows:
 - a) By changing the zoning from "AA" (Agricultural) to "RT-30-xx" (Street - Townhouse) District, Modified, to the lands the extent and boundaries of which are shown on Schedule "A"; and

- b) By changing the zoning from "C" (Urban Protected Residential, etc.) to "RT-30-xx" (Street - Townhouse) District, Modified, to the lands the extent and boundaries of which are shown on Schedule "A".
2. That the following special provisions apply to "RT-30-xx";
 - a) That notwithstanding any provision of the Zoning By-Law No. 6593, for the purposes of this By-law, a common element condominium road is deemed to be a public street.
 - b) That notwithstanding Sub-section 10F.(3) of the Zoning By-Law No. 6593, a maximum building height of two (2) storeys shall be provided and maintained.
 - c) That notwithstanding Sub-section 10F.(4)(a) of the Zoning By-Law No. 6593, a minimum front yard depth of 3.2m shall be provided and maintained.
 - d) That notwithstanding Sub-section 10F.(4)(b) of the Zoning By-Law No. 6593, a minimum rear yard depth of at least 6.0m shall be provided and maintained.
 - e) That notwithstanding Sub-section 10F.(5)(b) of the Zoning By-Law No. 6593, a minimum building separation distance of 2.4m shall be provided and maintained.
 - f) That notwithstanding Sub-section 10F.(6)(i) of the Zoning By-Law No. 6593, a minimum lot area of 150 m² shall be provided and maintained for each single-family dwelling.
 - g) That notwithstanding Sub-section 18A.(1)(a) of the Zoning By-Law No. 6593, a minimum of 2 parking spaces per dwelling unit shall be provided and maintained.
 - h) That notwithstanding Sub-section 18A.(1)(b) of the Zoning By-Law No. 6593, a minimum of 0.25 visitor parking spaces per dwelling unit shall be provided and maintained.
 - i) That notwithstanding Sub-section 18A.(17) of the Zoning By-Law No. 6593, a minimum parking stall size of 2.8m in width and 5.8m in length shall be provided and maintained.
 3. That no building or structure shall be erected, altered, extended or enlarged, nor shall any building or structure or part thereof be used, nor shall any land be used, except in accordance with the "RT-30" District provision, subject to the special requirements in Section 2 of this By-law.
 4. That Sheet No. E38d of the District maps be amended by making the lands referred to in Section 1 of this By-law as "RT-30-xx" (Street - Townhouse) District, Modified.
 5. That the Clerk is hereby authorized and directed to proceed with the giving of notice of the passing of this By-law, in accordance with the *Local Planning Appeal Tribunal Act*.

PASSED this _____ , 2021

F. Eisenberger
Mayor

Andrea Holland
City Clerk

Schedule "A"
By-Law No. _____
City of Hamilton



This is a Schedule "A"
To By-Law No. _____ Passed
This ___ Day of ___, 2021



ADDRESS : 513 Locust Street, Unit B
Burlington, Ontario
L7S 1V3

PHONE : (905) 681-1769

EMAIL : glenn@wellingsplanning.ca
brad@wellingsplanning.ca

WEBSITE : wellingsplanning.ca